Stephen K. Yamashiro

Mayor



Virginia Goldstein

Director

Russell Kokubun
Deputy Director

County of Hawaii

PLANNING DEPARTMENT

25 Aupuni Street, Room 109 • Hilo, Hawaii 96720-4252 (808) 961-8288 • Fax (808) 961-8742 Received

DEC 1 1 1998

December 9, 1998

Oceans

Robert W. Cunningham, LPLS Belt Collins Hawaii 680 Ala Moana Blvd., First Floor Honolulu, HI 96813-5406

Dear Mr. Cunningham:

TENTATIVE APPROVAL
SUBDIVIDER: 1250 OCEANSIDE PARTNERS
"THE VILLAGES OF HOKUKANO, PHASE 1"
Proposed Consolidation of TMK: 8-1-04:Portion 3 and
Resubdivision into 387 Lots plus Roadways and
Designation of Easements at
Honuaino 4, Hokukano 1&2, Kanaueue 1&2, Halekii,
Keekee 1&2, Ilikahi, Kanakau 1&2, Kalukalu 1,2&3, and
Onouli 1, North and South Kona, Island of Hawaii, Hawaii

TMK: 8-1-004:003 (SUB 98-124)

Please be informed that tentative approval of the preliminary plat map dated October 2, 1998, is hereby granted with modifications and conditions.

The subdivider is now authorized to prepare detailed drawings of the subdivision plan in accordance with Chapter 23, Subdivision Control Code, County of Hawaii, as modified. Before final approval can be granted, the following conditions must be met:

- 1. Provide a water system meeting with the approval of the Department of Water Supply.
- 2. Submit water system construction plans for approval by affected agencies.

- 3. Pay installation and facilities charges as required by the Department of Water Supply.
- 4. Identify all watercourses and drainage ways and encumber with drainage easements on final plat.
- 5. Portions of the subdivision are located within Zones "AE", "VE", "A" and "Shaded X" according to the current Flood Insurance Rate Map (FIRM).
 - a. Identify the Zones "AE", "VE" and "Shaded X" boundaries and areas on the final plat.
 - b. Provide adequate drainage to reduce exposure to flood damage. Provide drainage facilities and easements as necessary. Provide drainage calculations for review and comment.
 - c. Determine the base (100 year) flood elevations for the Zone "A" within the subdivision. Submit a flood study, prepared by a licensed civil engineer, for review and comment.
 - d. Identify the FIRM Zone "A" boundaries and base flood elevations on the final plat.
 - e. Submit a Flood Study to obtain a Federal Emergency Management Agency (FEMA) approved Conditional Letter of Map Revision (CLOMR) prior to any improvements within the Flood Zones. File application for Letter of Map Revision (LOMR) with FEMA upon completion of the flood study, as required.
- 6. A wastewater disposal system shall be constructed in a manner meeting with the approval of the State Department of Health and/or the Department of Public Works, which ever is applicable.
- 7. Submit a Solid Waste Management Plan to the Department of Public Works for review and comment.

- 8. Provide a 10-ft. wide "no vehicular access" planting screen easement fronting Road Lots A, C and the future Bypass Road Lot. The Department of Public Works will not allow individual lot access onto arterial and major collector roadways.
- 9. Extend Road Lots A and D to the northern boundary of the subdivision.
- 10. Provide access to Lot 354.
- 11. Roadway design including allowable street grades and minimum curve radii shall conform to the standards of the subdivision code.
- 12. Align Road Lots A-1 & D, A-2 & B, A-4 & J, H and I to intersect at the same location or provide intersection spacing in conformance with the subdivision code.
- 13. Streets shall be laid out to intersect at right angles and/or radially along curves except where topography requires a lesser angle, but the angle shall not be less than sixty degrees unless there is a special intersection design.
- 14. Pursuant to the conditions of the Change of Zone Ordinance Nos. 96-7, 96-8 and 97-36, the subdivider shall satisfy the following requirements, prior to Final Subdivision Approval. In lieu of the actual construction of the improvements identified below, the subdivider may enter into an agreement to assure the County that the improvements will be constructed together with the appropriate bond, surety or other security as may be approved by the County.
 - a. Construct the channelization and signalization of the Mamalahoa Highway-Haleki'i Street intersection meeting with the approval of Department of Public Works.
 - b. Construct the extension of Haleki'i Street (Road Lots A and K) meeting the requirements of the Department of Public Works. The phasing of the extension of the Haleki'i Street shall be in accordance with Paragraph 16 of Development Agreement No. 1.

- c. Construct the Mamalahoa Highway Bypass in its entirety between the approximate vicinity of Keauhou and Captain Cook, consisting of two lanes with sufficient right-of-way for a total of four lanes meeting with the approval of the Department of Public Works in accordance with the requirements of Paragraph 13 of Development Agreement No. 1.
- 15. The Mamalahoa Highway Bypass shall be dedicated to the County in accordance with Paragraph 14 of Development Agreement No. 1. The Haleki'i Street Extension shall be dedicated to the County upon its completion.
- 16. For Road Lot C, construct minimum 24-foot wide dedicable pavement within a minimum 60-ft. wide right-of-way meeting with the approval of the Department of Public Works or as may otherwise be approved in accordance with the Subdivision Code.
- 17. For all minor streets, including Road Lots A-1, B, B-1, B-2, B-3, B-4, B-6, C-1, C-2, C-4, C-5, C-7, C-9, D, E, E-2, F, G, G-1, H, H-1, H-3, H-4, I, I-1 and J, construct minimum 20-ft. wide dedicable pavement within a minimum 50-ft. wide right-of-way meeting with the approval of Department of Public Works or as may otherwise be approved in accordance with the Subdivision Code.
- 18. The terminus of Road Lot F shall be improved to provide a turnaround area meeting with the approval of Department of Public Works. This turnaround area may be part of the improvements for the Coastline Park which is being provided in accordance with Paragraph 18 of Development Agreement No. 1.
- 19. Construct turnarounds at the terminus of proposed cul-de-sacs meeting with the approval of Department of Public Works or as may otherwise be approved in accordance with the Subdivision Code.

Robert W. Cunningham, LPLS Page 5 December 9, 1998

20. For Road Lots serving 6 or less lots on a dead-end street, the requirements relating to non-dedicable private dead end streets may be complied with. In the alternative, construct 20-ft. wide dedicable pavement within a 50-ft. wide right-of-way meeting with the approval of Department of Public Works or as may otherwise be approved in accordance with the Subdivision Code.

Submit construction plans and drainage report for review and comment.

- a. Additional storm runoff due to development shall be disposed within the subdivision and shall not be discharged onto adjacent properties or roadways. For planned drywells, satisfy Department of Health (DOH) drywell requirements, including issuance of an underground injection control (UIC) permit to the subdivider.
- b. Install streetlights/signs/pavement markings as required by the Traffic Division, Department of Public Works.
- c. Identify all private roadways on the construction plans.
- 21. Comply with all conditions of approved Change of Zone Ordinances: REZ 765(Ord. 94 73); REZ 812(Ord. 96 7); REZ 846(Ord. 97 36); State Land Use Boundary Amendment 959 (Ord. 97 35); Special Management Area (SMA) Major Permits 345 & 356; Use Permit No. 115; General Plan 19; and Development Agreement No. 1.
- 22. Submit written proof that all taxes and assessments on the property are paid to date.
- 23. Place property markers in accordance with the final plat map. Surveyor shall submit certification upon completion.

Robert W. Cunningham, LPLS Page 6 December 9, 1998

- 24. Submit nine (9) copies of the final plat map prepared in conformity with Chapter 23, Subdivisions, within one year from the date of tentative approval, on or before December 9, 1999. If not, tentative approval to the preliminary plat map shall be deemed null and void. Only upon written request from the subdivider and for a good cause can a time extension be granted, provided it is submitted forty-five (45) days before the expiration of said period of one year.
- 25. Subdivider shall complete all requirements specified as conditions for tentative approval of the preliminary plat map within three (3) years of said tentative approval, on or before December 9, 2001. An extension of not more than two (2) years may be granted by the director upon timely request of the subdivider.

As part of final plat map submittal, the Planning Director requests an additional copy of the final plat map be submitted as a ".dwg" or ".dxf" diskette file prepared by CAD software.

Please submit a proposed street name conforming to the adopted street naming policy of the County of Hawaii.

Please be aware that if at any time during the fulfillment of the foregoing conditions, should concerns emerge such as environmental problems or other problems which were earlier overlooked or not anticipated/accounted for in data/reports available to date, this could be sufficient cause to immediately cease and desist from further activities on the proposed subdivision, pending resolution of the problems. The Planning Director shall confer with the listed officers to resolve the problems and notify you accordingly.

No final approval for recordation shall be granted until all the above conditions have been met.

Land shall not be offered for sale, lease or rent until final approval for recordation of the subdivision.

Robert W. Cunningham, LPLS Page 7 December 9, 1998

Should you have any questions, please feel free to contact Ed Cheplic of this department.

Sincerely,

VIRGINIA GOLDSTEIN

Planning Director

ETC:lnm 98124125.ten

Enc.- PPM (10-2-98)

xc:

Manager, DWS

Chief Engineer, DPW

District Environmental Health Program Chief, DOH

District Engineer, DOT

PLNG-KONA

DPW-ENG-KONA

1250 Oceanside Partners

William L. Moore





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BELT COLLINS HAWAII

County of Malvaii

PLANNING DEPARTMENT

25 Aupuni Street, Room 109 • Hilo, Hawaii 96720-4252 (808) 961-8288 • Fax (808) 961-8742

July 27, 1999

Robert W. Cunningham, LPLS Belt Collins Hawaii 680 Ala Moana Blvd., First Floor Honolulu, HI 96813-5406

Dear Mr. Cunningham:

REVISED TENTATIVE APPROVAL
SUBDIVIDER: 1250 OCEANSIDE PARTNERS
"HOKULI'A, PHASES 1 AND 2"
Proposed Consolidation of TMK: 8-1-04:56 & Portion 3
And Resubdivision into Lots 1 to 377, Inclusive
And Roadways and Designation of Easements at
Honuaino 4, Hokukano 1&2, Kanaueue 1&2, Halekii,
Keekee 1&2, Ilikahi, Kanakau 1&2, Kalukalu 1,2&3, and
Onouli 1, North and South Kona, Island of Hawaii, Hawaii
TMK: 8-1-004:056 & Portion of 003 (SUB 98-124)

Peg J. Hirota, P. Agena, L Maces. A. Water bereiter F time. A. Konto, C. Les. L Myssim, E Onuma, T. Palesh, C. Saratra, K. Tyzu, C. YOKOLL, A. Yeshiraga, J. 3-th 225

Please be informed that Revised Tentative Approval of the revised preliminary plat map dated June 29, 1999, is hereby granted with modifications and conditions.

The subdivider is now authorized to prepare detailed drawings of the subdivision plan in accordance with Chapter 23, Subdivision Control Code, County of Hawaii, as modified. Before final approval can be granted, the following conditions must be met:

- 1. Provide a water system meeting with the approval of the Department of Water Supply.
- 2. Submit water system construction plans for approval by affected agencies.

- 3. Pay installation and facilities charges as required by the Department of Water Supply.
- 4. Identify all watercourses and drainage ways and encumber with drainage easements on final plat.
- 5. Portions of the subdivision are located within Zones "AE", "VE", "A" and "Shaded X" according to the current Flood Insurance Rate Map (FIRM).
 - a. Identify the Zones "AE", "VE" and "Shaded X" boundaries and areas on the final plat.
 - Provide adequate drainage to reduce exposure to flood damage.
 Provide drainage facilities and easements as necessary. Provide drainage calculations for review and comment.
 - c. Determine the base (100 year) flood elevations for the Zone "A" within the subdivision. Submit a flood study, prepared by a licensed civil engineer, for review and comment.
 - d. Identify the FIRM Zone "A" boundaries and base flood elevations on the final plat.
 - e. Submit a Flood Study to DPW (Department of Public Works) for a CLOMR (Conditional Letter of Map Revision) from FEMA (Federal Emergency Management Agency) prior to construction plan approval. File application for LOMR (Letter of Map Revision) with FEMA upon completion of construction.
 - 6. Submit a Solid Waste Management Plan to the Department of Public Works for review and comment.
 - 7. Provide a 10-ft. wide "no vehicular access" planting screen easement for all 1± acre lots fronting Road Lots A, C and the Future Bypass Road Lot. The Department of Public Works will not allow individual lot access onto arterial and major collector roadways.

- 8. Recommend that stub-outs be provided at all boundaries of the subdivision.
- 9. Roadway design including allowable street grades and minimum curve radii shall conform to the standards of the code.
- 10. Telescoping of roadways is not acceptable. Provide proper roadway tapers in accordance with the "Hawaii Statewide Uniform Design Manual for Streets and Highways."
- 11. All roadway intersections shall intersect at right angles and/or radially along curves except where topography requires a lesser angle, but the angle shall not be less than sixty degrees unless there is a special intersection design.
- Pursuant to the conditions of the Change of Zone Ordinance Nos. 96-7, 96-8 and 97-36, the subdivider shall satisfy the following requirements, prior to Final Subdivision Approval:
 - a. Construct the channelization and signalization of the Mamalahoa Highway-Haleki'i Street intersection meeting with the approval of Department of Public Works.
 - b. Construct the extension of Haleki'i Street (Road Lot A) meeting the requirements of the Department of Public Works.
 - c. Construct the Mamalahoa Highway Bypass in its entirety between the approximate vicinity of Keauhou and Captain Cook, consisting of two lanes with sufficient right-of-way for a total of four lanes meeting with the approval of the Department of Public Works. Construction and dedication of the Mamalahoa Bypass shall be in accordance with the Development Agreement (Resolution No. 244 98).
 - 13. All roadway improvements as specified in item nos. 12a and 12b above shall be dedicated to the County of Hawaii as required in Condition M of Change of Zone Ordinance No. 96 8.

- 14. For Road Lot C, construct minimum 20-foot wide dedicable pavement within a minimum 60-ft. wide right-of-way meeting with the approval of the Department of Public Works.
- 15. For all minor streets serving more than six lots, construct minimum 20-ft. wide dedicable pavement within a minimum 50-ft. wide right-of-way meeting with the approval of Department of Public Works.
- 16. Construct turnarounds at the terminus of proposed cul-de-sacs meeting with the approval of Department of Public Works.
- 17. For Road Lots serving 6 lots or less, construct minimum 16-ft. wide nondedicable pavement within a minimum 20-ft. width right-of-way meeting with the approval of Department of Public Works.
- 18. Submit construction plans and drainage report for review and comment.
 - a. Additional storm runoff due to development shall be disposed within the subdivision and shall not be discharged onto adjacent properties or roadways. For planned drywells, satisfy Department of Health (DOH) drywell requirements.
 - b. Install streetlights/signs/pavement markings as required by the Traffic Division, Department of Public Works.
 - c. Identify all private roadways on the construction plans.
- 19. Comply with all conditions of approved Change of Zone Ordinances: REZ 765(Ord. 94 73); REZ 812(Ord. 96 7); REZ 846(Ord. 97 36); State Land Use Boundary Amendment 959 (Ord. 97 35); Special Management Area (SMA) Major Permits 345 & 356; Use Permit No. 115; General Plan 19; and Development Agreement No. 1.
- 20. Submit written proof that all taxes and assessments on the property are paid to date.

Robert W. Cunningham, LPLS Page 5 July 27, 1999

- 21. Place property markers in accordance with the final plat map. Surveyor shall submit certification upon completion.
- 22. Submit nine (9) copies of the final plat map prepared in conformity with Chapter 23, Subdivisions, within one year from the date of Revised Tentative Approval, on or before July 27, 2000. If not, Revised Tentative Approval to the revised preliminary plat map shall be deemed null and void. Only upon written request from the subdivider and for a good cause can a time extension be granted, provided it is submitted forty-five (45) days before the expiration of said period of one year.
- 23. Subdivider shall complete all requirements specified as conditions for Revised Tentative Approval of the revised preliminary plat map within three (3) years of said Revised Tentative Approval, on or before July 27, 2002. An extension of not more than two (2) years may be granted by the director upon timely request of the subdivider.

As part of final plat map submittal, the Planning Director requests an additional copy of the final plat map be submitted as a ".dwg" or ".dxf" diskette file prepared by CAD software.

Please submit a proposed street name conforming to the adopted street naming policy of the County of Hawaii.

Please be aware that if at any time during the fulfillment of the foregoing conditions, should concerns emerge such as environmental problems or other problems which were earlier overlooked or not anticipated/accounted for in data/reports available to date, this could be sufficient cause to immediately cease and desist from further activities on the proposed subdivision, pending resolution of the problems. The Planning Director shall confer with the listed officers to resolve the problems and notify you accordingly.

No final approval for recordation shall be granted until all the above conditions have been met.

Land shall not be offered for sale, lease or rent until final approval for recordation of the subdivision.

Robert W. Cunningham, LPLS Page 6 July 27, 1999

In conclusion, based on this Revised Tentative Approval action, our Tentative Approval of December 9, 1998, is now deemed Null and Void.

Should you have any questions, please feel free to contact Ed Cheplic of this department.

Sincerely,

VIRGINIA GOLDSTEIN

Planning Director

ETC:lnm

98124125.rtn

Enc.- Rev.PPM (6-29-99)

xc:

Manager, DWS

Chief Engineer, DPW

District Environmental Health Program Chief, DOH

District Engineer, DOT

PLNG-KONA

DPW-ENG-KONA

1250 Oceanside Partners

William L. Moore

REZ 812(Ord. 96 7), REZ 765(Ord. 96 8), REZ 846(Ord. 97 36)

Development Agreement No. 1